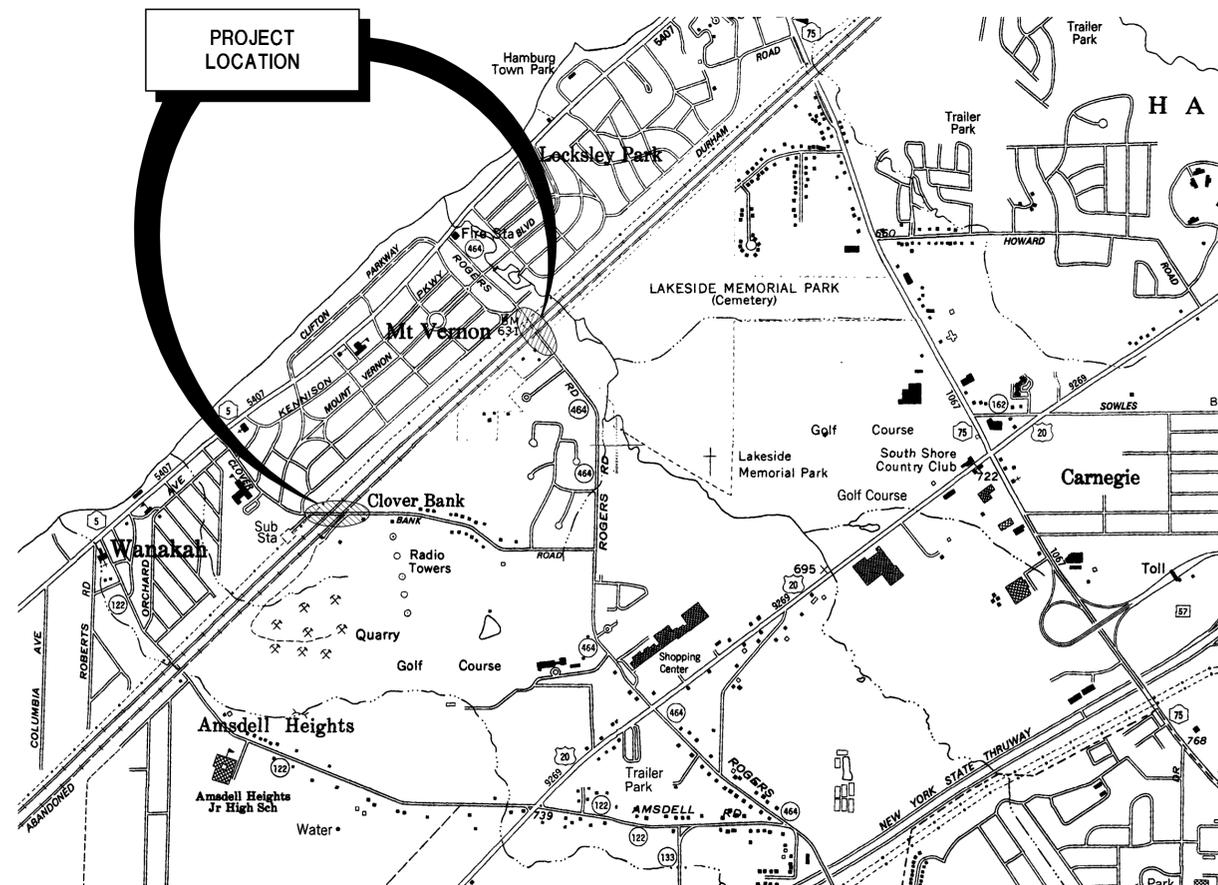




CONTRACT DRAWINGS FOR ROGERS ROAD & CLOVERBANK ROAD RAILROAD QUIET ZONES

TOWN OF HAMBURG, ERIE COUNTY

P.I.N. 5759.70



LOCATION MAP
N.T.S.

SCOPE OF WORK:

GRADING, DRAINAGE, ASPHALT CONCRETE PAVEMENT REHABILITATION, AND TRAFFIC SEPARATOR SYSTEM INSTALLATION.

PROJECT LOCATION:

ROGERS ROAD AND CLOVERBANK ROAD ARE LOCATED IN THE TOWN OF HAMBURG, COUNTY OF ERIE. THE PROJECT IS LOCATED AT THE CSX TRANSPORTATION AND NORFOLK SOUTHERN RAILROAD CROSSINGS AT ROGERS ROAD AND CLOVERBANK ROAD AND EXTENDS APPROXIMATELY 150 FEET EAST AND WEST OF THE CROSSINGS.

SPECIFICATIONS:

ALL WORK CONTEMPLATED UNDER THIS CONTRACT IS TO BE COVERED BY AND IN CONFORMITY WITH THE NEW YORK STATE DEPARTMENT OF TRANSPORTATION STANDARD SPECIFICATIONS FOR CONSTRUCTION AND MATERIALS OF MAY 1, 2008 EXCEPT AS MODIFIED ON THESE PLANS AND IN THE ITEMIZED PROPOSAL.

NYSDOT STANDARD SHEETS:

203-01, 203-02, 209-01, 209-02, 209-03, 209-04, 209-05, 209-07, 402-01, 619-04, 619-10, 619-11, 619-12, 619-20, 619-60, 619-61, 645-01, 645-03, 645-14, 646-15, 685-01

MAINTENANCE AND PROTECTION OF TRAFFIC:

TRAFFIC IS TO BE MAINTAINED BY THE CONTRACTOR FOR THE DURATION OF THE LIMITS OF WORK AS SHOWN ON THE PLANS. PAYMENT FOR THIS WORK WILL BE MADE UNDER THE APPROPRIATE ITEM OF SECTION 619, WORK ZONE TRAFFIC CONTROL.

MAINTENANCE JURISDICTION:

OWNERSHIP AND MAINTENANCE JURISDICTION FOR CLOVERBANK ROAD IS WITH THE TOWN OF HAMBURG AND WILL REMAIN SO AFTER COMPLETION OF THE PROJECT. ROGERS ROAD IS PLANNED TO BE TRANSFERRED FROM THE JURISDICTION OF ERIE COUNTY TO THE TOWN OF HAMBURG PRIOR TO COMPLETION OF THE PROJECT.

HIGHWAY FUNCTIONAL CLASSIFICATION:

ROGERS ROAD: URBAN MAJOR COLLECTOR
CLOVERBANK ROAD: URBAN LOCAL

DESIGN VEHICLE:

SINGLE UNIT TRUCK (SU)

PROJECTED TRAFFIC:

DESIGN YEAR: 2034
ROGERS ROAD: 8,331 VPD
CLOVERBANK ROAD: 2,508 VPD

DESIGN SPEED:

ROGERS ROAD: 35 MPH
CLOVERBANK ROAD: 30 MPH

TRUCK VOLUME:

ROGERS ROAD: 5.0%
CLOVERBANK ROAD: 5.0%

DRAWING INDEX		
SHEET NO.	DESCRIPTION	DRAWING NO.
1	COVER	-
2	ABBREVIATIONS, LEGEND & SUMMARY OF QUANTITIES	AL-1
3	WORK ZONE TRAFFIC CONTROL	WZ-1
4	TYPICAL SECTIONS	TS-1
5-6	CONSTRUCTION DETAILS & TABLES	CD-1 THRU CD-2
7-10	ROADWAY PLANS & PROFILES	PL-1 THRU PL-4
11-12	SIGN & PAVEMENT MARKING PLANS	PM-1 THRU PM-2



09.03.13
DATE

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ROGERS ROAD			
CLOVERBANK ROAD			
ERIE COUNTY			
FED. ROAD REG. NO.	STATE	SHEET NO.	TOTAL SHEETS
1	N.Y.	1	12
FEDERAL AID PROJECT NO.	FA PROJECT		
CAPITAL PROJECT IDENTIFICATION NO.	5759.70		
CONTRACT NO.			

DESIGN SUPERVISOR D. ZIEMANSKI JOB MANAGER D. ELIAS DESIGNED BY J. ZHAO CHECKED BY D. ELIAS ESTIMATED BY J. ZHAO DRAFTED BY J. ZHAO CHECKED BY D. ELIAS

DESIGN SUPERVISOR D. ZIEMANSKI JOB MANAGER D. ELIAS DESIGNED BY J. ZHAO CHECKED BY D. ELIAS ESTIMATED BY J. ZHAO DRAFTED BY J. ZHAO CHECKED BY J. ZHAO DRAFTED BY J. ZHAO CHECKED BY D. ELIAS

A. GENERAL NOTES

1. THE TYPICAL DETAILS DEPICTED ON THE STANDARD SHEETS AND IN THE MUTCD REFLECT THE MINIMUM REQUIREMENTS.
2. THE CONTRACTOR MUST SUBMIT TO THE ENGINEER, IN WRITING, PROPOSED REVISIONS TO THE TRAFFIC CONTROL PLAN FOR REVIEW AND APPROVAL BY THE HIGHWAY SUPERINTENDENT OR HIS/HER DESIGNEE FIVE (5) DAYS PRIOR TO THE PLANNED IMPLEMENTATION OF SUCH PROPOSED REVISIONS, EXCEPT FOR CHANGES THAT ALTER THE SCOPE OF THE TRAFFIC CONTROL PLAN. SUCH CHANGES IN SCOPE MUST BE SUBMITTED TO THE ENGINEER FOR APPROVAL BY THE HIGHWAY SUPERINTENDENT OR HIS/HER DESIGNEE THIRTY (30) WORKING DAYS PRIOR TO IMPLEMENTATION OF SUCH REVISIONS.
3. THE CONTRACTOR SHALL PROVIDE THE ENGINEER, IN WRITING, WITH THE NAMES, ADDRESSES, AND TELEPHONE NUMBERS OF STAFF WHO ARE AUTHORIZED TO SECURE LABOR, MATERIALS, AND EQUIPMENT FOR EMERGENCY REPAIRS OUTSIDE NORMAL WORKING HOURS. THE ENGINEER WILL PROVIDE THE SUBMITTED INFORMATION TO EMERGENCY SERVICE PROVIDERS
4. PRIOR TO THE START OF ANY WORK OPERATIONS, ALL RELATED WORK FOR PROPOSED WORK ZONE TRAFFIC CONTROL SHALL BE COMPLETE. THIS INCLUDES BUT IS NOT LIMITED TO, ALL SIGNS, SIGNALS, PAVEMENT MARKINGS, BARRIERS, DELINEATION (CONES, DRUMS, ETC.), FLAGGERS, PAVEMENT MODIFICATIONS, AND ANY OTHER RELATED WORK.
5. THE TRAFFIC CONTROL PLANS SHOWN ARE TO BE CONSIDERED MINIMUM REQUIREMENTS. ADDITIONAL SIGNS AND/OR TRAFFIC CONTROL DEVICES MAY BE REQUIRED AS DETERMINED BY THE ENGINEER, COST TO BE INCLUDED IN THE PRICE BID FOR THE APPROPRIATE ITEMS. IF AT ANY TIME THE ENGINEER DETERMINES THAT TRAFFIC IS NOT BEING PROPERLY MAINTAINED WITHIN A WORK ZONE, THE CONTRACTOR SHALL IMMEDIATELY CORRECT THE INDICATED DEFICIENCY.
6. WEEDS, SHRUBBERY, CONSTRUCTION MATERIALS, EQUIPMENT, VEHICLES, OR WORKERS SHALL NOT OBSCURE TRAFFIC CONTROL DEVICES OR OBSTRUCT TRAFFIC. THE CONTRACTOR SHALL BE REQUIRED TO TRIM ANY FOLIAGE OBSTRUCTING THE VISIBILITY OF ANY TRAFFIC CONTROL DEVICE WHETHER PERMANENT, TEMPORARY, OR CONSTRUCTION. THE COST OF THIS WORK SHALL BE INCLUDED IN THE PRICE BID FOR BASIC WORK ZONE TRAFFIC CONTROL.
7. THE CONTRACTOR SHALL COORDINATE ANY WORK WITH OTHER CONTRACTORS, UTILITY COMPANIES, MUNICIPALITIES, AND PERMITEES IN ACCORDANCE WITH STANDARD SPECIFICATION 105-06, TO ENSURE ALL TRAFFIC IS PROPERLY AND SAFELY MAINTAINED.

B. SURFACE CONDITION

1. TEMPORARY SLOPED TAPERS REQUIRED PER STANDARD SPECIFICATION 619-3.02 SHALL BE CONSTRUCTED FROM BITUMINOUS PATCHING MATERIAL (PLANT MIX OR SUITABLE WINTER MIX). THESE TAPERS SHALL BE MAINTAINED AND REMAIN IN PLACE UNTIL THE NEED FOR TAPERING IS ELIMINATED BY SUBSEQUENT WORK. THE COST FOR THIS WORK SHALL BE INCLUDED IN THE PRICE BID FOR BASIC WORK ZONE TRAFFIC CONTROL.
2. A W8-1 "BUMP" SIGN SHALL BE PLACED AT THE APPROPRIATE ADVANCE WARNING DISTANCE ON ALL APPROACHES WHERE A BUMP IS PRESENT IN THE ROADWAY.
3. WHEN MAINTAINING TRAFFIC ON PAVED OR MILLED SURFACES, THE CONTRACTOR SHALL MARK ALL HAZARDS PROJECTING UP FROM THE DRIVING SURFACE (I.E. VALVES, MANHOLES, ETC.). THE CONTRACTOR SHALL PROVIDE TEMPORARY SLOPED TAPERS FOR ALL VALVES, MANHOLES, ETC. LOCATED WITHIN THE TRAVEL LANES. THE CONTRACTOR IS RESPONSIBLE FOR PLACEMENT AND REMOVAL OF THESE TAPERS. TEMPORARY TAPERS SHALL BE CONSTRUCTED FROM BITUMINOUS PATCHING MATERIAL (PLANT MIX OR SUITABLE WINTER MIX). FOR VALVES, MANHOLES, ETC. LOCATED OUTSIDE THE ACTUAL TRAVEL LANES (I.E. SHOULDERS, CENTERLINE) THE CONTRACTOR MAY MARK THE FEATURE AS A HAZARD WITH A DRUM, IF TAPERING IS NOT POSSIBLE. ALL VALVES, MANHOLES, ETC. LOCATED WITHIN AN INTERSECTION SHALL BE TAPERED. THESE TAPERS SHALL BE MAINTAINED AND REMAIN IN PLACE UNTIL THE NEED FOR TAPERING IS ELIMINATED BY SUBSEQUENT WORK. THE COST OF THIS WORK, INCLUDING REMOVAL OF TAPERS, SHALL BE INCLUDED IN THE PRICE BID FOR BASIC WORK ZONE TRAFFIC CONTROL.

C. SIGNS

1. THE LOCATIONS OF THE SIGNS SHOWN ON THE WORK ZONE TRAFFIC CONTROL PLANS AND DETAILS MAY BE ADJUSTED BASED ON SIGHT DISTANCE AND OTHER CONSIDERATIONS. THE FINAL LOCATIONS OF SIGNS ARE SUBJECT TO APPROVAL BY THE ENGINEER.
2. ANY EXISTING SIGNS, INCLUDING OVERHEAD SIGNS, WHICH CONFLICT WITH THE TEMPORARY TRAFFIC CONTROL SIGN LAYOUT SHALL BE COVERED, REMOVED, STORED OR RESET, AS APPROVED BY THE ENGINEER. ALL APPROPRIATE EXISTING SIGNS SHALL BE RESTORED TO THEIR ORIGINAL CONDITION AND/OR LOCATION UNLESS OTHERWISE REPLACED IN THIS CONTRACT.
3. SIGNS AT OR NEAR INTERSECTIONS SHALL BE PLACED SO THAT THEY DO NOT OBSTRUCT A MOTORIST'S LINE OF SIGHT.
4. THE DIMENSIONS OF WORK ZONE TRAFFIC CONTROL SIGNS ARE DESCRIBED IN THE MUTCD. ANY CHANGES TO THE DIMENSIONS SHALL BE APPROVED BY THE REGIONAL DIRECTOR OR BY HIS/HER DESIGNEE.

D. CHANNELIZING DEVICES

1. WHERE POSSIBLE ALL CHANNELIZING AND GUIDING DEVICES ARE TO BE PLACED SO AS TO PROVIDE A MINIMUM 2' LATERAL CLEARANCE TO THE TRAVELED WAY.
2. CHANNELIZATION DEVICES USED IN TAPERS SHALL BE UNIFORM.
3. CHANNELIZATION DEVICES USED IN THE TANGENT SECTION OF A LANE CLOSURE OR SHOULDER CLOSURE SHALL BE UNIFORM.
4. THE CONTRACTOR SHALL USE DRUMS FOR NIGHT TIME LANE CLOSURES AND NIGHT TIME SHOULDER CLOSURES.

E. PUBLIC ACCESS

1. PROPERTY OWNERS WHOSE DRIVEWAYS WILL BE MADE INACCESSIBLE SHALL BE NOTIFIED BY THE CONTRACTOR AT LEAST 24 HOURS PRIOR TO RESTRICTING USE OF THE DRIVEWAY. FOR MULTIPLE ACCESS PROPERTIES, AT LEAST ONE DRIVEWAY SHALL BE OPEN AT ALL TIMES. ACCESS SHALL BE RESTORED TO ALL DRIVEWAYS AS SOON AS POSSIBLE.
2. SUITABLE RAMPS SHALL BE INSTALLED TO MAINTAIN SMOOTH TRANSITIONS FROM RESIDENTIAL AND COMMERCIAL DRIVEWAYS TO AND FROM THE WORK AREA.

F. LANE CLOSURES

1. THE CONTRACTOR SHALL LOCATE LANE CLOSURES TO PROVIDE OPTIMUM VISIBILITY, I.E. BEFORE CURVES AND CRESTS, TO THE EXTENT CONDITIONS PERMIT.
2. THE ENGINEER MAY REQUIRE THAT ALL LANES BE RE-OPENED AT ANY TIME IF THE ROUTE IS NEEDED FOR EMERGENCY PURPOSES. THIS COULD INCLUDE INCIDENTS AT LOCATIONS OUTSIDE THE CONTRACT LIMITS.

G. LANE WIDTHS

1. UNLESS AUTHORIZED BY THE ENGINEER, THE MINIMUM LANE WIDTHS FOR WORK ZONE TRAVEL LANE SHALL BE AS FOLLOWS: FREEWAYS AND/OR EXPRESSWAYS IS 11', THE MINIMUM LANE WITH FOR OTHER TYPES OF ROADWAYS IS 10'.
2. THE CONTRACTOR SHALL PROVIDE WRITTEN NOTICE TO THE ENGINEER A MINIMUM OF 21 CALENDAR DAYS IN ADVANCE OF PERFORMING ANY WORK THAT RESULTS IN THE REDUCED WIDTH OF AN EXISTING ROADWAY.

H. BARRIER/SHADOW VEHICLES

1. BARRIER AND SHADOW VEHICLES SHALL BE REQUIRED AS PER STANDARD SHEET TITLED "WORK ZONE TRAFFIC CONTROL LEGEND AND TABLES".
2. NO WORK ACTIVITY, EQUIPMENT, VEHICLES AND/OR MATERIALS SHALL BE LOCATED BETWEEN THE BARRIER OR SHADOW VEHICLE AND THE ACTIVE WORK AREA (ROLL AHEAD DISTANCE).
3. A SHADOW VEHICLE IS REQUIRED FOR SETTING UP OF TEMPORARY TRAFFIC CONTROL SIGNS AND DEVICES WHERE WORKERS ARE IN A LANE OR SHOULDER EXPOSED TO TRAFFIC.
4. WHEN MULTIPLE WORK OPERATIONS WITHIN THE SAME WORK ZONE ARE SEPARATED BY A LONGITUDINAL DISTANCE GREATER THAN 360 FEET, EACH OPERATION SHALL BE CONSIDERED AN INDEPENDENT WORK AREA AND SHALL BE SUBJECT TO ALL BARRIER VEHICLE REQUIREMENTS REGARDLESS OF THE PRESENCE OF BARRIER VEHICLES AT UPSTREAM WORK OPERATIONS.

I. NYSDOT STANDARD SHEETS

1. THE FOLLOWING NYSDOT STANDARD SHEETS APPLY TO THIS PROJECT:
 619-04: PORTABLE TEMPORARY WOODEN SIGN SUPPORT
 619-10: WORK ZONE TRAFFIC CONTROL GENERAL NOTES
 619-11: WORK ZONE TRAFFIC CONTROL LEGENDS AND NOTES
 619-12: SIGN TABLE
 619-20: SHOULDER CLOSURE
 619-60: FLAGGING OPERATION 2-LANE 2-WAY ROADWAY
 619-61: FLAGGING OPERATION 2-LANE 2-WAY ROADWAY INTERSECTIONS

J. PAYMENT ITEMS

1. THE FOLLOWING NYSDOT PAYMENT ITEMS APPLY TO THIS PROJECT:
 619.01 BASIC WORK ZONE TRAFFIC CONTROL
 619.100101 INTERIM PAVEMENT MARKINGS, STRIPES (TRAFFIC PAINT)
 619.0801 REMOVE EXISTING PAVEMENT MARKING STRIPES
 619.0802 REMOVE EXISTING PAVEMENT MARKING LETTERS OR SYMBOLS

K. PROJECT SPECIFIC

1. THE CONTRACTOR SHALL COORDINATE ALL WORK OPERATIONS WITH THE REGIONAL TRANSIT SERVICE IN ORDER TO MINIMIZE SERVICE DISRUPTIONS.

M. CONSTRUCTION VEHICLES AND EQUIPMENT

1. THE CONTRACTOR SHALL PLAN AND INCORPORATE ACCESS POINTS INTO THE WORK ZONE SUCH THAT, TO THE EXTENT PRACTICAL, THE CONTRACTOR'S VEHICLES ENTERING AND LEAVING THE WORK ZONE SHALL NOT IMPEDE THE MOVEMENT OF THROUGH TRAFFIC IN THE ADJACENT OPEN LANES.

N. PAVEMENT MARKINGS

1. TECHNIQUES FOR REMOVAL OR COVERING OF PAVEMENT MARKINGS ARE SUBJECT TO APPROVAL BY THE ENGINEER.
2. EXISTING PAVEMENT MARKINGS WITHIN THE CONTRACT LIMITS SHALL BE MAINTAINED AND RE-STRIPED IF REQUIRED BY THE ENGINEER.
3. INTERIM PAVEMENT MARKINGS STRIPES SHALL BE PLACED ON THE TOP COURSE BEFORE PLACEMENT OF THE FINAL PAVEMENT MARKINGS A.O.B.E.

O. NOTIFICATIONS AND EMERGENCY ACCESS

1. THE CONTRACTOR IS REQUIRED TO CONTACT ALL SCHOOLS AND EMERGENCY RESPONSE AGENCIES THAT MAY BE AFFECTED BY WORK ASSOCIATED WITH THIS PROJECT TO NOTIFY THEM OF ANTICIPATED CHANGES IN TRAVEL CONDITIONS. IN ADDITION, THESE ORGANIZATIONS SHALL BE UPDATED EACH TIME PROJECT ACTIVITIES CHANGE AND NEW TRAVEL CONDITIONS ARE CREATED. NOTIFICATIONS AND UPDATES SHALL BE PROVIDED AT LEAST TWO WEEKS PRIOR TO THE ALTERATION OF TRAVEL CONDITIONS.
2. THE CONTRACTOR IS REQUIRED TO CONTACT ALL TRANSIT ORGANIZATIONS THAT MAY BE AFFECTED BY LANE CLOSURES, DETOURS, OR ANY OTHER WORK ASSOCIATED WITH THIS PROJECT, TO NOTIFY THEM OF ANTICIPATED ALTERATIONS OF TRAFFIC CONDITIONS. IN ADDITION, THESE ORGANIZATIONS SHALL BE UPDATED EACH TIME PROJECT ACTIVITIES CHANGE AND NEW TRAVEL CONDITIONS ARE CREATED. NOTIFICATIONS AND UPDATES SHALL BE PROVIDED TO TRANSIT ORGANIZATIONS AT LEAST FOUR WEEKS PRIOR TO THE ALTERATION OF TRAVEL CONDITIONS. THE CONTRACTOR SHALL ALSO MAKE PROVISIONS FOR THE SAFE INGRESS AND EGRESS OF PASSENGERS AT EXISTING OR TEMPORARILY RELOCATED BUS STOPS.

P. TIME RESTRICTIONS & DATE RESTRICTIONS

1. THE FOLLOWING TIME RESTRICTIONS APPLY TO THIS PROJECT. NO WORK REQUIRING THE CLOSING OF LANES OR SIDE STREETS SHALL BE PERMITTED AT THE FOLLOWING TIMES:
 • ROGERS ROAD AND CLOVERBANK ROAD FROM 2:30PM TO SUNRISE
 • ROGERS ROAD AND CLOVERBANK ROAD FROM SUNSET TO 8:30AM

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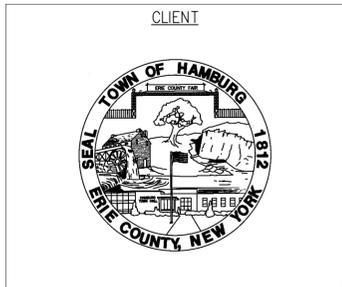
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DATE DATE

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REVISIONS			
NO.	DESCRIPTION	BY	DATE

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PROJECT NAME
ROGERS ROAD & CLOVERBANK ROAD RAILROAD QUIET ZONES

DRAWING TITLE
WORK ZONE TRAFFIC CONTROL
 NOTES

SCALE N.T.S.	DATE 9/3/2013
P.I.N. 5759.70	EAA PROJECT NO. 19406.00
SHEET NO. 3 OF 12	DRAWING NO. WZ-1

DESIGN SUPERVISOR D. ZIEMIANSKI JOB MANAGER D. ELIAS DESIGNED BY J. ZHAO CHECKED BY D. ELIAS ESTIMATED BY J. ZHAO DRAFTED BY J. ZHAO CHECKED BY D. ELIAS

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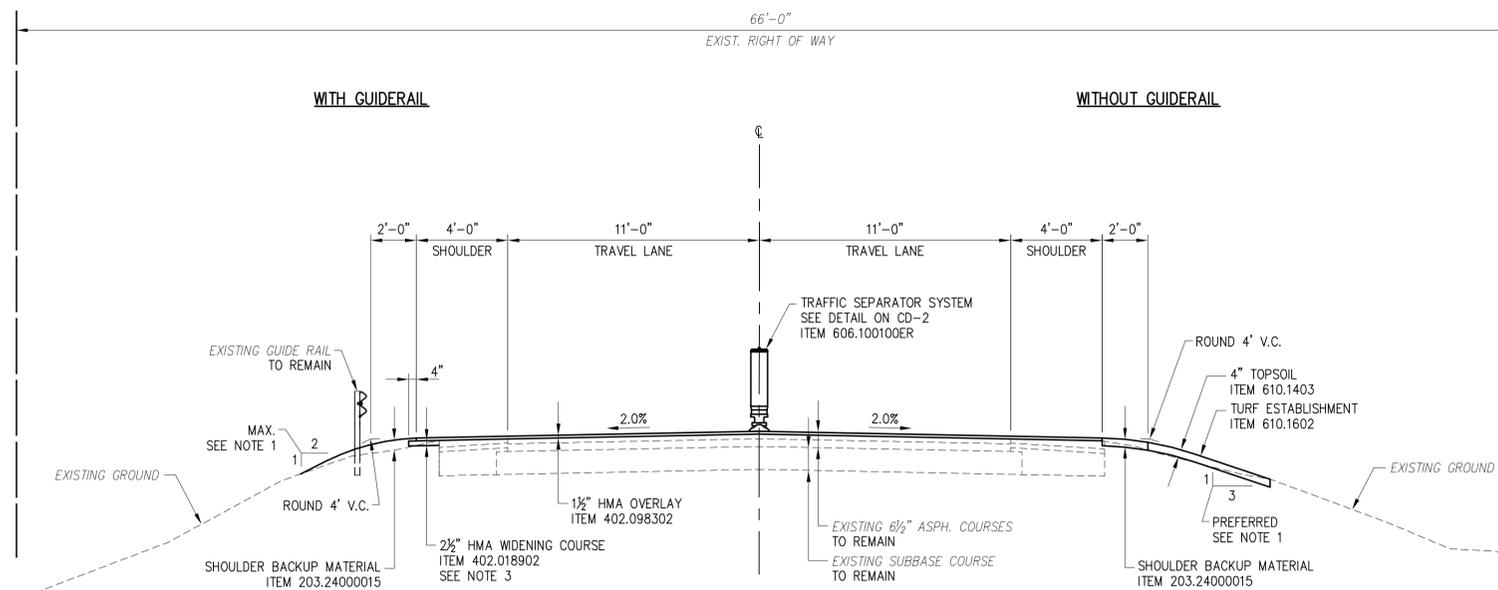


PROJECT NAME
ROGERS ROAD & CLOVERBANK ROAD RAILROAD QUIET ZONES

DRAWING TITLE

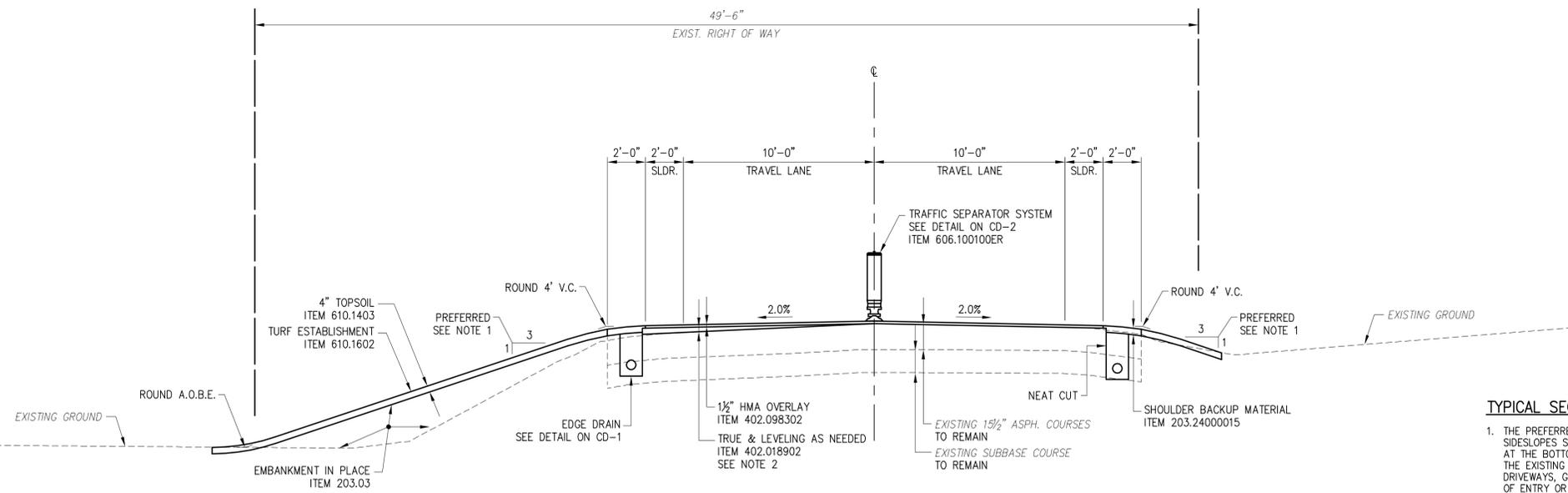
TYPICAL SECTIONS

SCALE AS NOTED	DATE 9/3/2013
P.I.N. 5759.70	EAA PROJECT NO. 19406.00
SHEET NO. 4 OF 12	DRAWING NO. TS-1



ROGERS ROAD

STA. 16+50 TO STA. 21+20
¼"=1'-0"



CLOVERBANK ROAD

STA. 11+00 TO STA. 16+75
¼"=1'-0"

TYPICAL SECTION NOTES:

- THE PREFERRED SIDESLOPE TREATMENT INVOLVES FLATTENING OF EXISTING SIDESLOPES STEEPER THAN 1:3 AND PROVISION OF A CLEAR RUNOUT AREA AT THE BOTTOM OF THE FILL SLOPES. DOING SO MAY REQUIRE WORK BEYOND THE EXISTING HIGHWAY BOUNDARY OR EASEMENTS TO RE-ESTABLISH DRIVEWAYS, GRADE & RE-ESTABLISH TURF, OR REMOVE/TRIM TREES. A RIGHT OF ENTRY OR PERMISSION FORM WILL BE REQUIRED FROM EACH PROPERTY OWNER. IF THE RELEASE IS NOT GRANTED FROM THE PROPERTY OWNERS, THE PROPOSED WORK SHALL TERMINATE AT THE EXISTING HIGHWAY BOUNDARY OR EASEMENT LIMIT. WORK SHALL NOT COMMENCE BEYOND THE HIGHWAY BOUNDARY UNTIL SO DIRECTED BY THE ENGINEER.
- A TRUING & LEVELING COURSE MAY BE REQUIRED IN SPOT LOCATIONS ON CLOVERBANK ROAD IN ORDER TO CORRECT PAVEMENT CROSS SLOPE IN EXCESS OF 3% OR LESS THAN 1.5%. THE ENGINEER WILL ASSIST THE CONTRACTOR WITH THE LAYOUT OF ANY LOCATIONS REQUIRING A T&L COURSE. A NOMINAL QUANTITY OF 30 TONS OF ITEM 402.018902 HAS BEEN INCLUDED IN THE CONTRACT FOR THIS PURPOSE.
- THE EXISTING WESTBOUND SHOULDER FROM STA. 15+40 TO 19+50 ON ROGERS ROAD SHALL BE WIDENED TO ACCOMMODATE THE PROPOSED 4'-0" SHOULDER WIDTH. THE EXISTING SHOULDER BACKUP MATERIAL SHALL BE TRIMMED AND FINE GRADED AS NEEDED TO ALLOW FOR PLACEMENT OF THE 2½" HMA COURSE. NO SEPARATE MEASUREMENT OR PAYMENT WILL BE MADE FOR PREPARATION OF THE SURFACE.
- PAVEMENT REPAIRS MAY BE REQUIRED IN SPOT LOCATIONS ON CLOVERBANK ROAD IN ORDER TO CORRECT DEEP WHEELPATH RUTS OR POT HOLES PRIOR TO OVERLAYING THE EXISTING PAVEMENT. THE ENGINEER WILL ASSIST THE CONTRACTOR WITH THE LAYOUT OF ANY LOCATIONS REQUIRING SPOT REPAIRS.
- ALL EXISTING PAVEMENT AND MILLED SURFACES SHALL BE CLEANED UNDER ITEM 633.11 PRIOR TO APPLICATION OF TACK COAT.
- ALL MILLED SURFACES AND EXISTING PAVEMENT TO BE OVERLAID SHALL RECEIVE AN APPLICATION OF ITEM 407.0102 DILUTED TACK COAT PRIOR TO PAVING.

DESIGN SUPERVISOR D. ZIEMANSKI JOB MANAGER D. ELIAS DESIGNED BY J. ZHAO CHECKED BY D. ELIAS ESTIMATED BY J. ZHAO DRAFTED BY J. ZHAO CHECKED BY D. ELIAS

SIGN TEXT DATA							
TEXT NUMBER	LOCATION NUMBER	ITEM NUMBER	PAYMENT FACTOR	TOTAL QUANTITY	TEXT	MUTCD NUMBER	MOUNT
1	1, 8, 10, 17	645.5102	15.00 SF	60.00 SF		W10-1 36" DIA.	GR MTD
		645.81	2 EA	8 EA		W10-9P 30" x 24"	
2	2, 3, 6, 7, 11, 13, 14, 16	645.5102	6.00 SF	48.00 SF		NYR9-5 24" x 30"	GR MTD
		645.81	1 EA	8 EA			

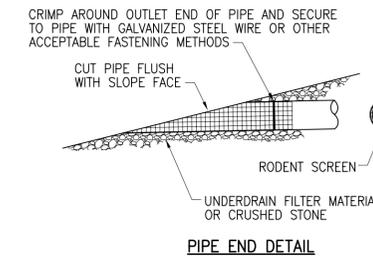
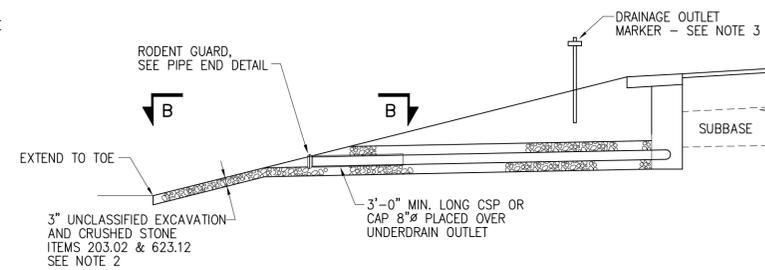
SIGN REMOVALS				
ITEM 647.51 - REMOVE AND DISPOSE SIGN PANEL, SIGN PANEL ASSEMBLY SIZE 1 (UNDER 30 SQUARE FEET)				
LOCATION NUMBER	SIDE	TEXT	ITEM 647.51 (EA)	
1	RT.	GRADE CROSSING ADVANCE WARNING SIGN	1	
4	RT.	DO NOT STOP ON TRACKS SIGN	1	
5	LT.	DO NOT STOP ON TRACKS SIGN	1	
8	LT.	GRADE CROSSING ADVANCE WARNING SIGN	1	
9	RT.	GRADE CROSSING ADVANCE WARNING SIGN	1	
12	RT.	STATE LAW SIGN	1	
15	LT.	STATE LAW SIGN	1	
18	LT.	GRADE CROSSING ADVANCE WARNING SIGN	1	
TOTAL:			8	

EDGE DRAIN TABLE							
ITEM 605.0901 - UNDERDRAIN FILTER, TYPE I (CY)							
ITEM 605.1502 - PERFORATED CORRUGATED POLYETHYLENE UNDERDRAIN TUBING, 6 INCH DIAMETER (LF)							
UPSTREAM STATION	SIDE	FEATURE	DOWNSTREAM STATION	SIDE	FEATURE	ITEM 605.0901	ITEM 605.1502
CLOVERBANK ROAD - NORTH SIDE							
12+67	LT	END CAP	11+00	LT	OUTLET	16.0	171
15+09	LT	END CAP	13+84	LT	OUTLET	13.0	139
15+39	LT	END CAP	15+74	LT	OUTLET	6.9	74
16+66	LT	END CAP	15+74	LT	OUTLET	6.5	69
CLOVERBANK ROAD - SOUTH SIDE							
12+38	RT	END CAP	11+37	RT	OUTLET	9.8	104
14+80	RT	END CAP	13+54	RT	DR	12.8	137
15+10	RT	END CAP	16+00	RT	DR	8.6	91
16+80	RT	END CAP	16+03	RT	DR	7.3	78
TOTAL						80.7	863

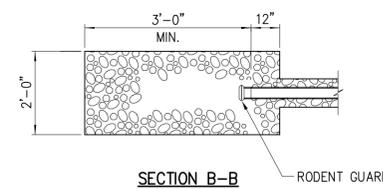
SIGN DATA LEGEND		
SYMBOL	DESCRIPTION	GENERAL NOTES
GR MTD OH MTD BR MTD PO MTD B-B	GROUND MOUNTED OVERHEAD MOUNTED BRIDGE MOUNTED POLE MOUNTED BACK TO BACK	1. THE FOLLOWING SHALL BE IN ACCORDANCE WITH THE MUTCD REQUIREMENTS FOR THE SPECIFIED MUTCD SIGN CODE: A. LETTER SIZE AND SERIES B. LEGEND & BACKGROUND COLOR C. REFLECTIVITY D. SIZE AND SHAPE OF SIGN 2. THE TYPE OF CHARACTERS AS SPECIFIED IN THE STANDARD SPECIFICATIONS SHALL BE AS FOLLOWS: MUTCD CODE LETTER TYPE OF CHARACTER G & I IV R, P, W, & M IV OR V
	LOCATION NUMBER TEXT NUMBER	
	REMOVAL NUMBER	
	RELOCATION NUMBER TEXT NUMBER	

SIGN NOTES:

- SIGN LOCATIONS AS SHOWN ON PLANS ARE APPROXIMATE. THE CONTRACTOR SHALL INSTALL NEW SIGNS IN ACCORDANCE WITH THE MUTCD.
- THE PAYMENT FACTORS FOR SIGNS ARE FROM THE APPLICABLE STANDARD SHEETS OR SIGN FACE LAYOUTS.
- THE PAYMENT FACTOR FOR POSTS IS THE NUMBER OF POSTS PROVIDED PER INSTALLATION.
- THE TOTAL PAYMENT QUANTITY IS OBTAINED BY MULTIPLYING THE NUMBER OF LOCATIONS (SHOWN IN THE LOWER RIGHT CORNER OF THE LOCATIONS BLOCK) BY THE PAYMENT FACTOR.
- WHERE MULTIPLE PANELS ARE LOCATED ON ONE SIGN ASSEMBLY, PAYMENT WILL ONLY BE MADE FOR REMOVING ONE SIGN UNLESS THE COMBINED AREA IS GREATER THAN 10 S.F.

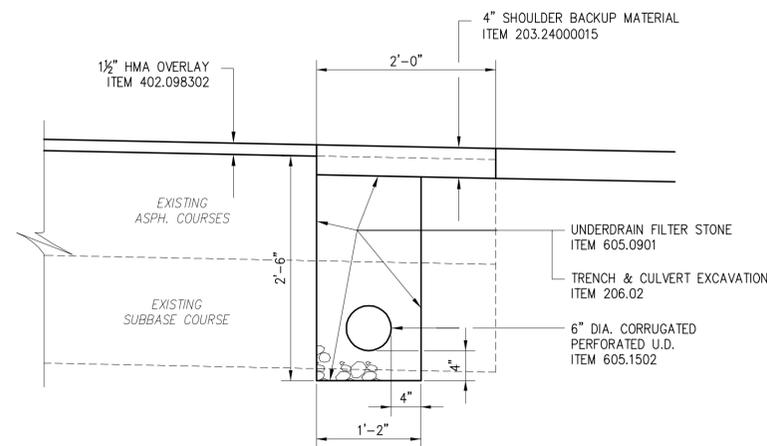
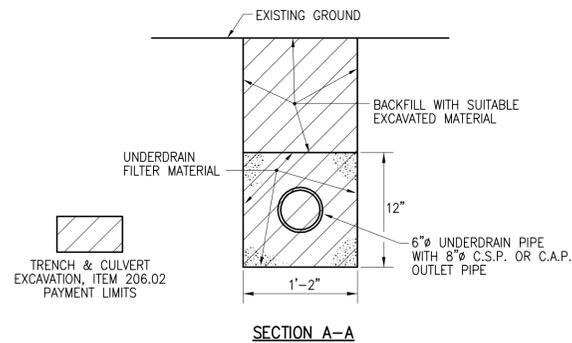


PROFILE

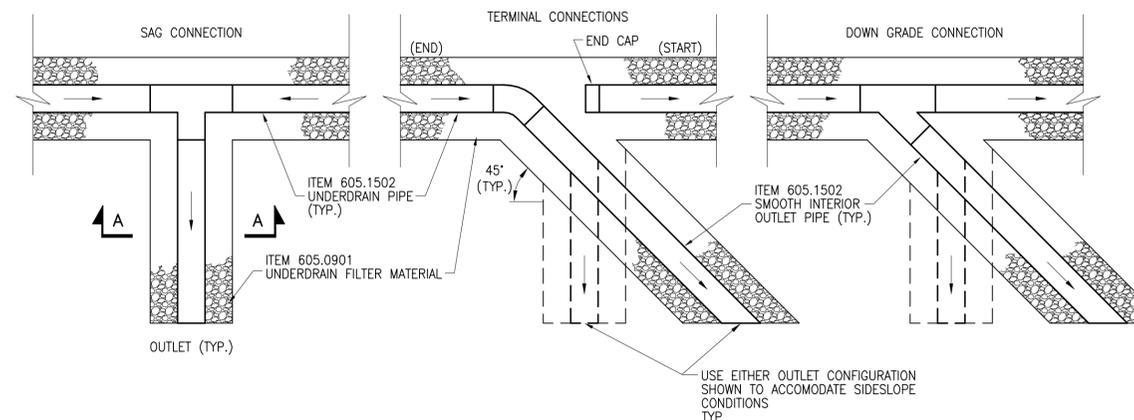


NOTES:

- GALVANIZED OR STAINLESS STEEL RIGID SCREENING WITH 1/4"-3/8" OPENINGS, 21 GAUGE OR HEAVIER, MAY BE SUBSTITUTED FOR A COMMERCIALLY MANUFACTURED RODENT GUARD A.O.B.E. ALL COSTS SHALL BE INCLUDED IN THE UNIT PRICE BID FOR UNDERDRAIN PIPE.
- CRUSHED STONE APRONS MAY BE OMITTED WHERE THE UNDERDRAIN PIPE OUTLETS TO A STONE DOWNSPOUT OR CULVERT APRON.
- DRAINAGE OUTLET MARKERS SHALL BE USED TO LOCATE ALL DRAINAGE OUTLETS EXCEPT THOSE OUTLETING TO DRAINAGE STRUCTURES. ALL COSTS SHALL BE PAID FOR UNDER ITEM 646.22 & ITEM 646.31.
- COST OF 8" C.S.P. OR C.A.P. SLEEVE SHALL BE INCLUDED IN THE UNIT PRICE BID FOR UNDERDRAIN PIPE.



EDGE DRAIN DETAIL
1" = 1'-0"



LATERAL OUTLET CONNECTIONS
EDGE DRAIN LATERAL OUTLET
NOT TO SCALE

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09.03.13
DATE DATE

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REVISIONS			
NO.	DESCRIPTION	BY	DATE

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PROJECT NAME
ROGERS ROAD & CLOVERBANK ROAD RAILROAD QUIET ZONES

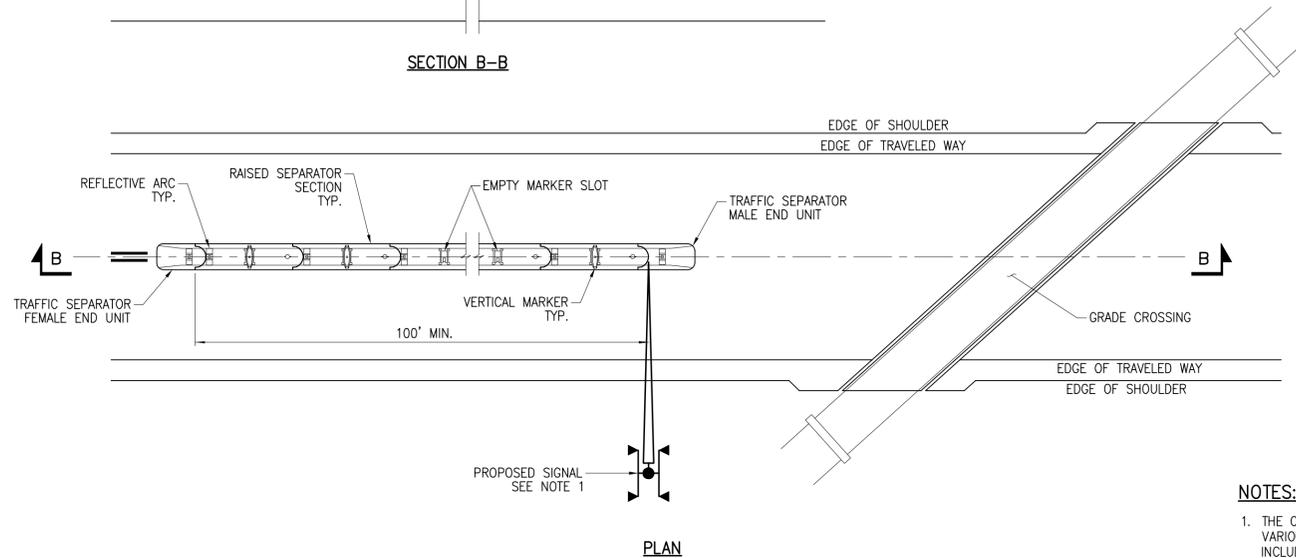
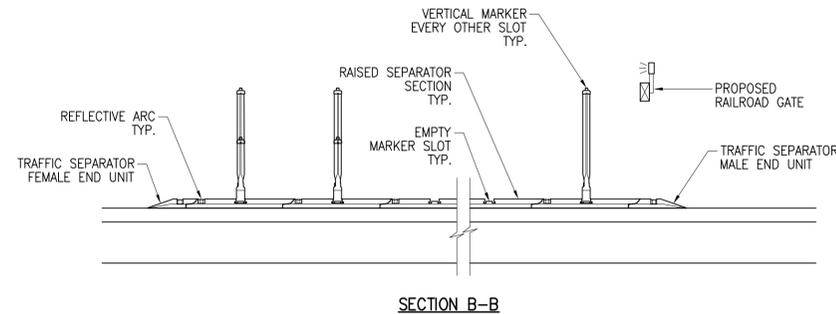
DRAWING TITLE
CONSTRUCTION DETAILS

SCALE AS NOTED	DATE 9/3/2013
P.I.N. 5759.70	EAA PROJECT NO. 19406.00
SHEET NO. 5 OF 12	DRAWING NO. CD-1

DESIGN SUPERVISOR D. ZIEMANSKI JOB MANAGER D. ELIAS
 DRAFTED BY J. ZHAO ESTIMATED BY D. ELIAS CHECKED BY J. ZHAO
 DRAFTED BY J. ZHAO CHECKED BY D. ELIAS
 DRAFTED BY J. ZHAO CHECKED BY D. ELIAS

NOTES:

1. THE PROPOSED SIGNAL STANCHION/GATE LOCATIONS SHOWN ON THE PLANS ARE APPROXIMATE. THE TRAFFIC SEPARATOR SYSTEM SHALL BE ADJUSTED BASED UPON AS-BUILT STANCHION/GATE LOCATIONS SO THAT THE END OF THE SEPARATOR CURB IS NO MORE THAN 1'-0" FROM THE GATE.
2. VERTICAL MARKERS ARE REQUIRED AT THE BEGINNING AND END OF THE RUN AS WELL AS AT EVERY OTHER MARKER SLOT. REFLECTIVE ARCS ARE REQUIRED AT EVERY ARC SLOT.
3. THE CONTRACTOR SHALL SUPPLY 20 SPARE VERTICAL MARKER ASSEMBLIES TO THE TOWN FOR FUTURE REPLACEMENT OF DAMAGED MARKERS. NO SEPARATE PAYMENT WILL BE MADE FOR THE SPARE MARKERS. ALL COSTS SHALL BE INCLUDED IN THE UNIT PRICE BID FOR ITEM 606.100100ER.

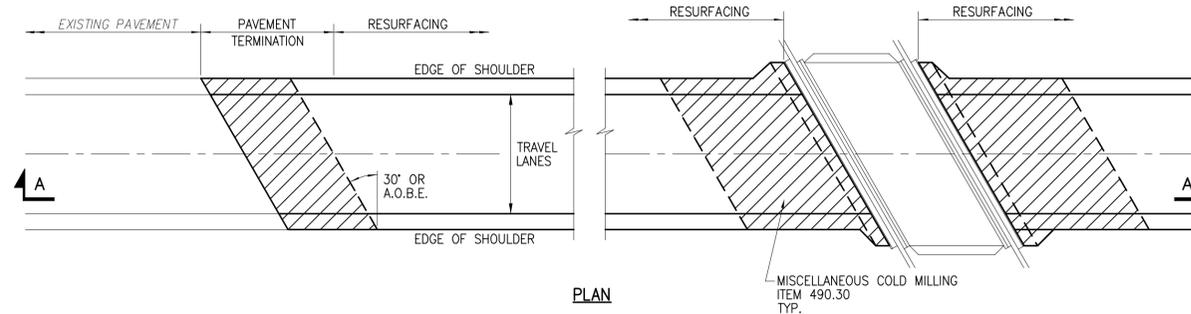
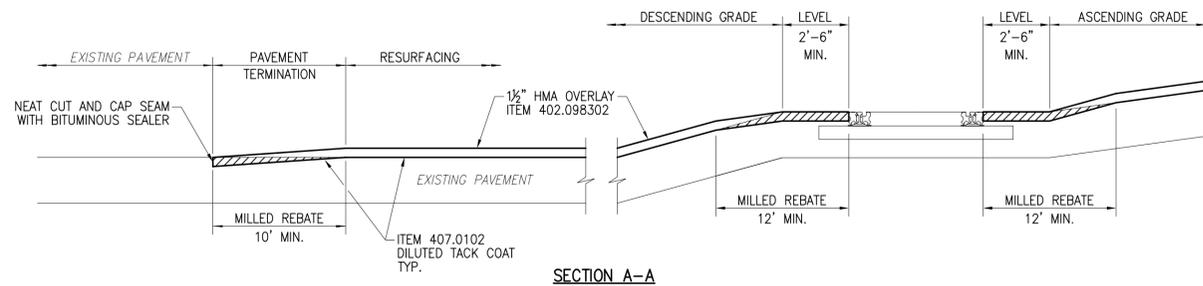


TRAFFIC SEPARATOR SYSTEM DETAIL
NOT TO SCALE

TRAFFIC SEPARATOR SYSTEM TABLE		
ITEM 606.100100ER - TRAFFIC SEPARATOR SYSTEM (LF)		
START STATION	END STATION	ITEM 606.100100ER
ROGERS ROAD		
16+90.0	17+93.0	103.0
18+41.0	19+54.0	112.9
19+78.5	20+82.0	103.0
CLOVERBANK ROAD		
11+25.0	12+28.0	103.0
13+46.0	14+65.5	119.7
15+55.5	16+58.5	103.0
TOTAL		644.6

NOTES:

1. THE COST OF SAWCUTTING AND CAPPING SEAM SHALL BE INCLUDED IN THE VARIOUS ASPHALT ITEMS. THE COST OF MILLING AND TACK COAT SHALL BE INCLUDED IN THEIR RESPECTIVE BID ITEMS.
2. MILLING EQUIPMENT SHALL NOT BE USED IMMEDIATELY ADJACENT TO THE GRADE CROSSING TIMBER HEADERS OR RUBBER RAIL SEALS. THE CONTRACTOR SHALL STOP MILLING AT LEAST 2" FROM THE TIMBER OR RUBBER AND CHIP OUT THE REMAINING ASPHALT BY HAND IN ORDER TO LIMIT DAMAGE TO THE CROSSING SURFACE.



PAVEMENT TERMINATION DETAIL
NOT TO SCALE

COLD MILLING TABLE		
ITEM 490.30 - MISCELLANEOUS COLD MILLING OF BITUMINOUS CONCRETE (SY)		
START STATION	END STATION	AREA (SY)
ROGERS ROAD		
16+40	16+50	33.3
17+89	18+01	41.5
18+33	18+45	41.6
19+52	19+64	40.7
19+70	19+82	40.7
21+20	21+30	33.3
CLOVERBANK ROAD		
10+90	11+00	30.4
12+51	12+63	32.9
13+11	13+23	32.9
14+93	15+05	32.9
15+14	15+26	32.9
16+75	16+85	26.5
TOTAL		419.8

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REVISIONS			
NO.	DESCRIPTION	BY	DATE

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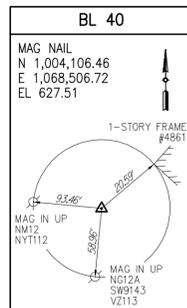
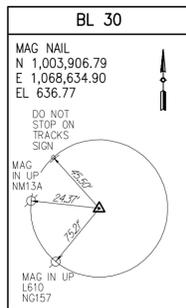
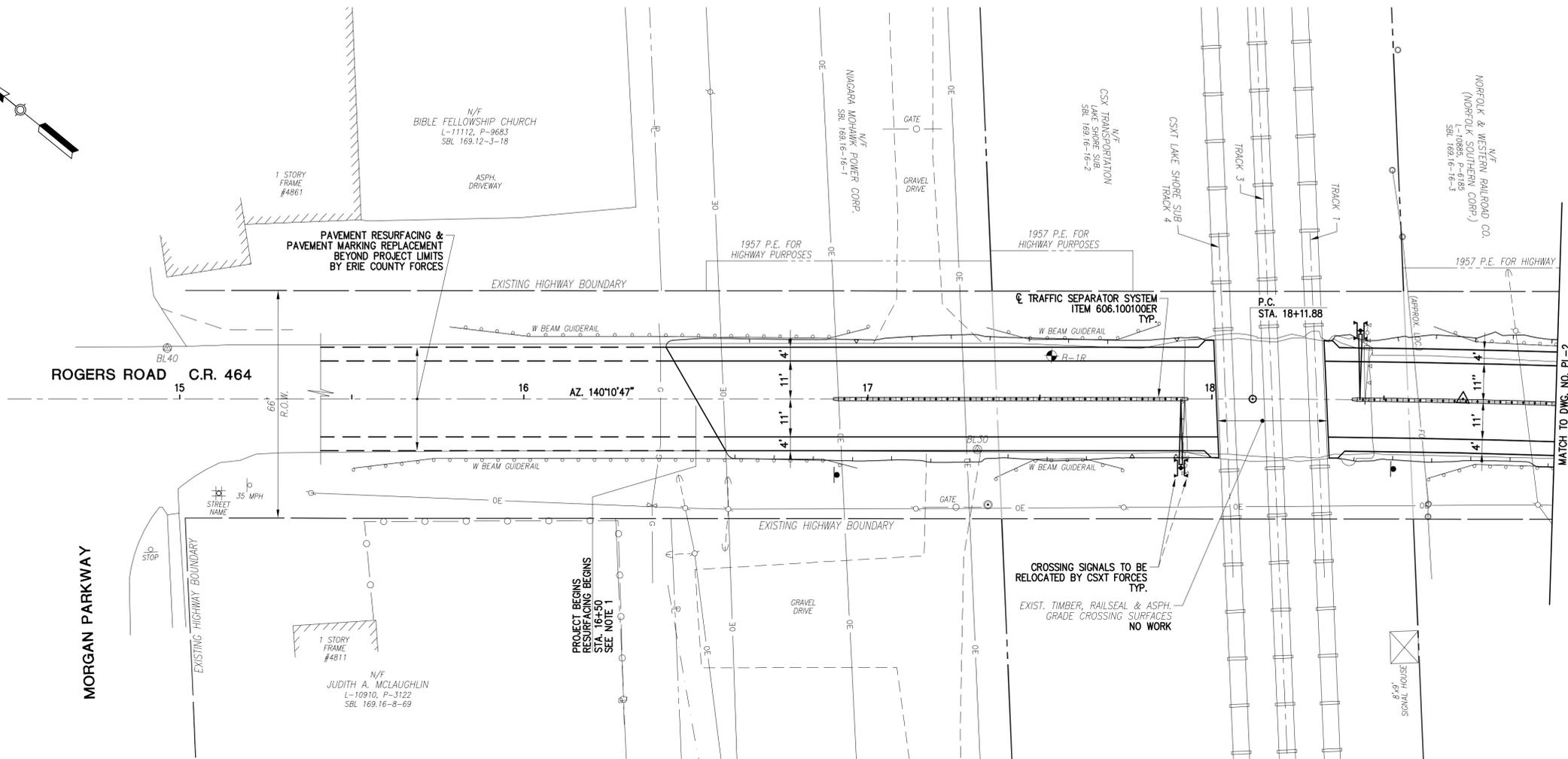
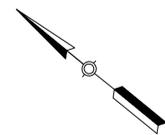


PROJECT NAME
ROGERS ROAD & CLOVERBANK ROAD RAILROAD QUIET ZONES

DRAWING TITLE
CONSTRUCTION DETAILS

SCALE AS NOTED	DATE 9/3/2013
P.I.N. 5759.70	EAA PROJECT NO. 19406.00
SHEET NO. 6 OF 12	DRAWING NO. CD-2

DESIGN SUPERVISOR D. ZIEMIANSKI JOB MANAGER D. ELIAS DESIGNED BY J. ZHAO CHECKED BY D. ELIAS ESTIMATED BY J. ZHAO DRAFTED BY J. ZHAO CHECKED BY J. ZHAO

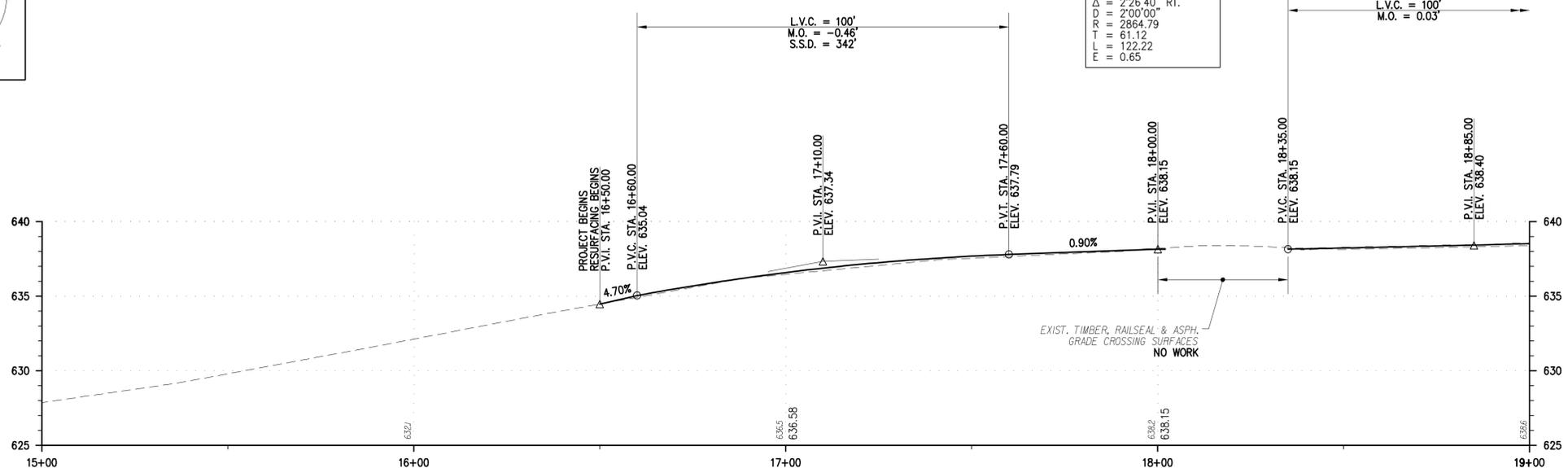


NOTES:

- PROJECT LIMITS ON ROGERS ROAD ARE APPROXIMATE. THE LIMITS WILL BE ADJUSTED BY THE ENGINEER IN ORDER TO OVERLAP THE REBATES WHICH WERE PREVIOUSLY MILLED BY THE ERIE COUNTY PROJECT TO ELIMINATE ANY DIPS OR BUMPS IN THE PROFILE.

CURVE 1 DATA

P.C. STA.	18+11.88
N	1,003,854.68
E	1,068,697.38
P.I. STA.	18+73.00
N	1,003,807.73
E	1,068,736.52
P.T. STA.	19+34.10
N	1,003,759.16
E	1,068,773.62
Δ	2°26'40" RT.
D	2'00'00"
R	2864.79
T	61.12
L	122.22
E	0.65



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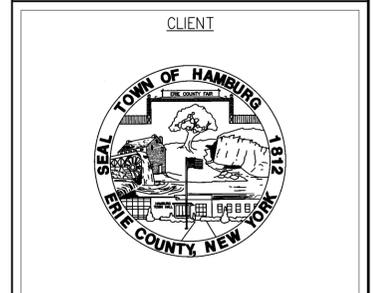


09.03.13
DATE DATE

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PROJECT NAME

ROGERS ROAD & CLOVERBANK ROAD RAILROAD QUIET ZONES

DRAWING TITLE

ROGERS ROAD PLAN & PROFILE

STA. 15+00 TO STA. 19+00

SCALE HORIZ: 1" = 20' VERT: 1" = 5'	DATE 9/3/2013
P.I.N. 5759.70	EAA PROJECT NO. 19406.00
SHEET NO. 7 OF 12	DRAWING NO. PL-1

DESIGN SUPERVISOR D. ZIEMIANSKI JOB MANAGER D. ELIAS DESIGNED BY J. ZHAO CHECKED BY D. ELIAS ESTIMATED BY J. ZHAO DRAFTED BY J. ZHAO CHECKED BY J. ZHAO

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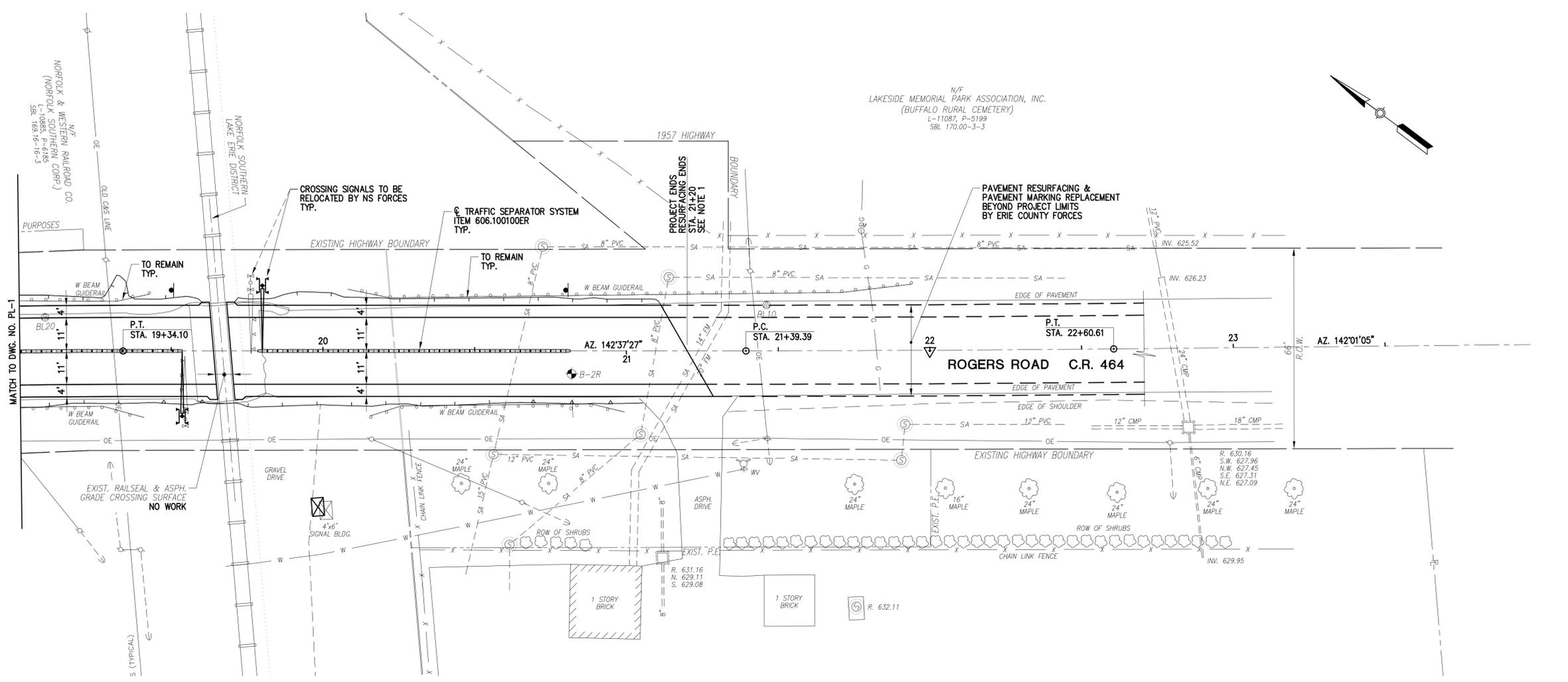
CLIENT

ROGERS ROAD & CLOVERBANK ROAD RAILROAD QUIET ZONES

DRAWING TITLE

ROGERS ROAD PLAN & PROFILE
STA. 19+00 TO STA. 24+00

SCALE HORIZ: 1" = 20' VERT: 1" = 5'	DATE 9/3/2013
P.I.N. 5759.70	EAA PROJECT NO. 19406.00
SHEET NO. 8 OF 12	DRAWING NO. PL-2

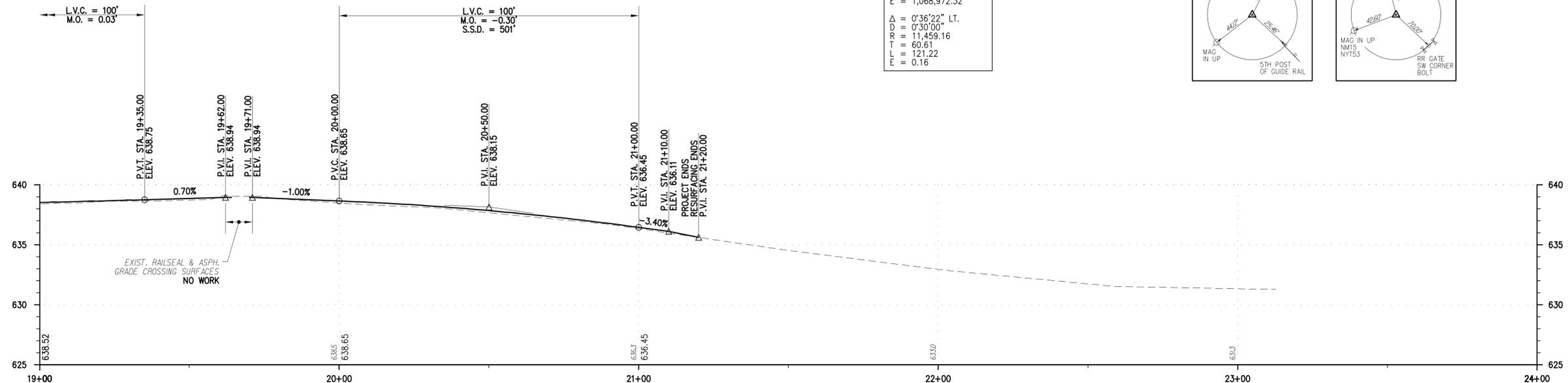
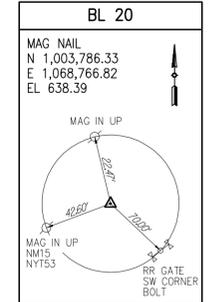
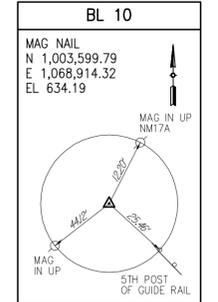


NOTES:

- PROJECT LIMITS ON ROGERS ROAD ARE APPROXIMATE. THE LIMITS WILL BE ADJUSTED BY THE ENGINEER IN ORDER TO OVERLAP THE REBATES WHICH WERE PREVIOUSLY MILLED BY THE ERIE COUNTY PROJECT TO ELIMINATE ANY DIPS OR BUMPS IN THE PROFILE.

CURVE 2 DATA

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E = 1,068,898.23
P.I. STA. 21+99.98
N = 1,003,547.88
E = 1,068,935.02
P.T. STA. 22+60.59
N = 1,003,500.10
E = 1,068,972.32
$\Delta = 0^\circ36'22"$ LT.
D = 0'30"00"
R = 11,459.16
T = 60.61
L = 121.22
E = 0.16



DESIGN SUPERVISOR D. ZIEMIANSKI JOB MANAGER D. ELIAS DESIGNED BY J. ZHAO CHECKED BY D. ELIAS ESTIMATED BY J. ZHAO CHECKED BY D. ELIAS DRAFTED BY J. ZHAO CHECKED BY J. ZHAO

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REVISIONS			
NO.	DESCRIPTION	BY	DATE

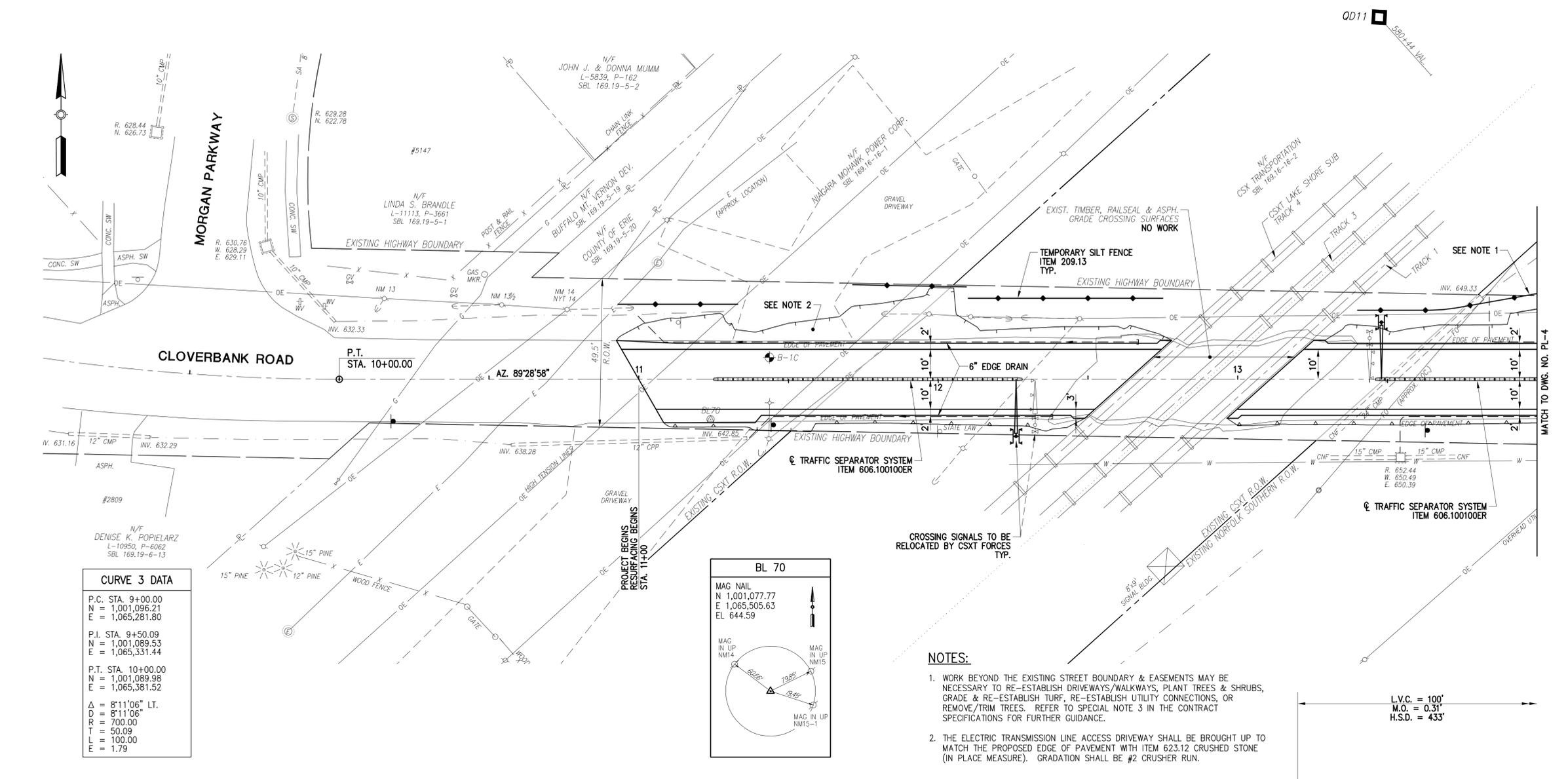
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PROJECT NAME
ROGERS ROAD & CLOVERBANK ROAD RAILROAD QUIET ZONES

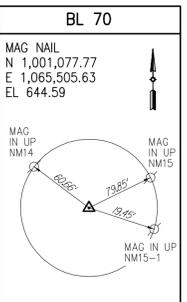
DRAWING TITLE
CLOVERBANK ROAD PLAN & PROFILE
STA. 9+00 TO STA. 14+00

SCALE HORIZ: 1" = 20' VERT: 1" = 5'	DATE 9/3/2013
P.I.N. 5759.70	EAA PROJECT NO. 19406.00
SHEET NO. 9 OF 12	DRAWING NO. PL-3



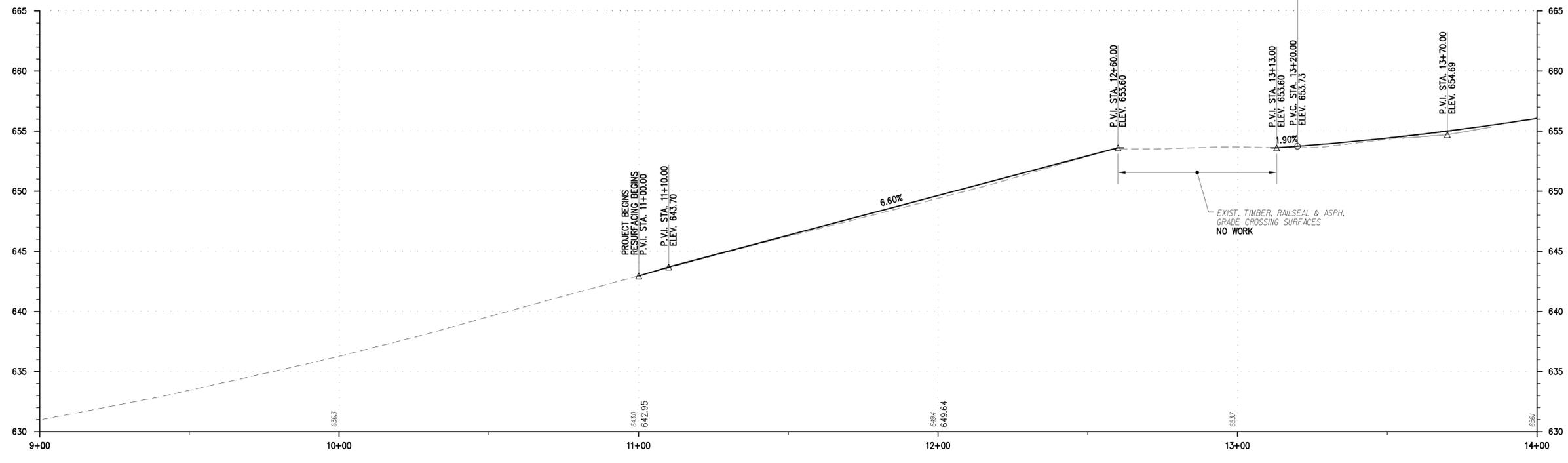
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P.I. STA. 9+50.09
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P.T. STA. 10+00.00
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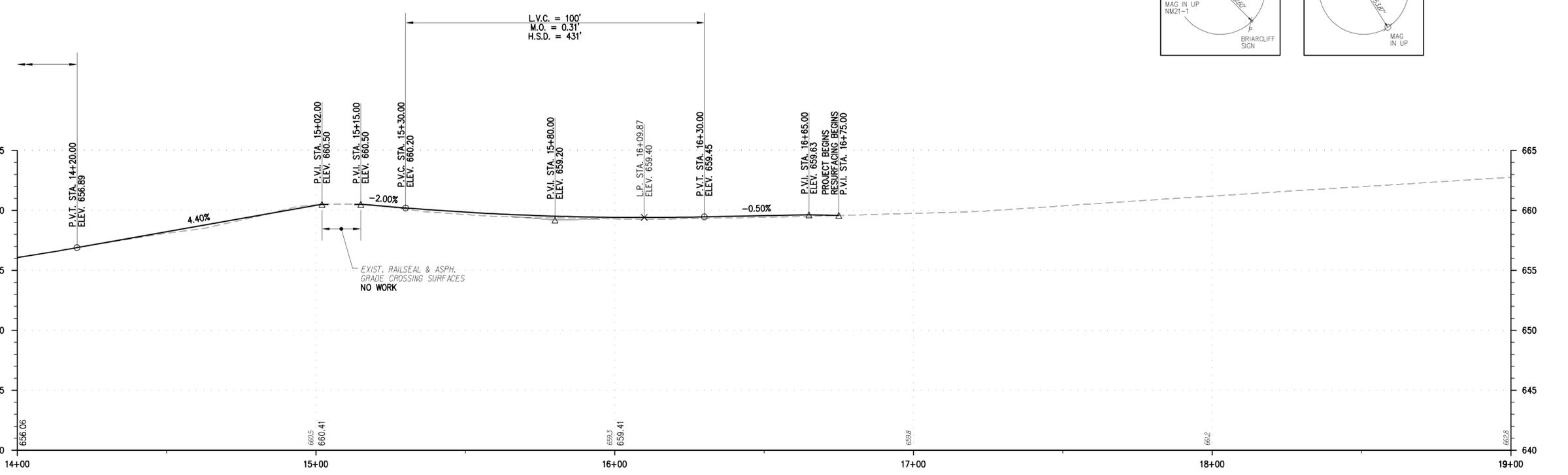
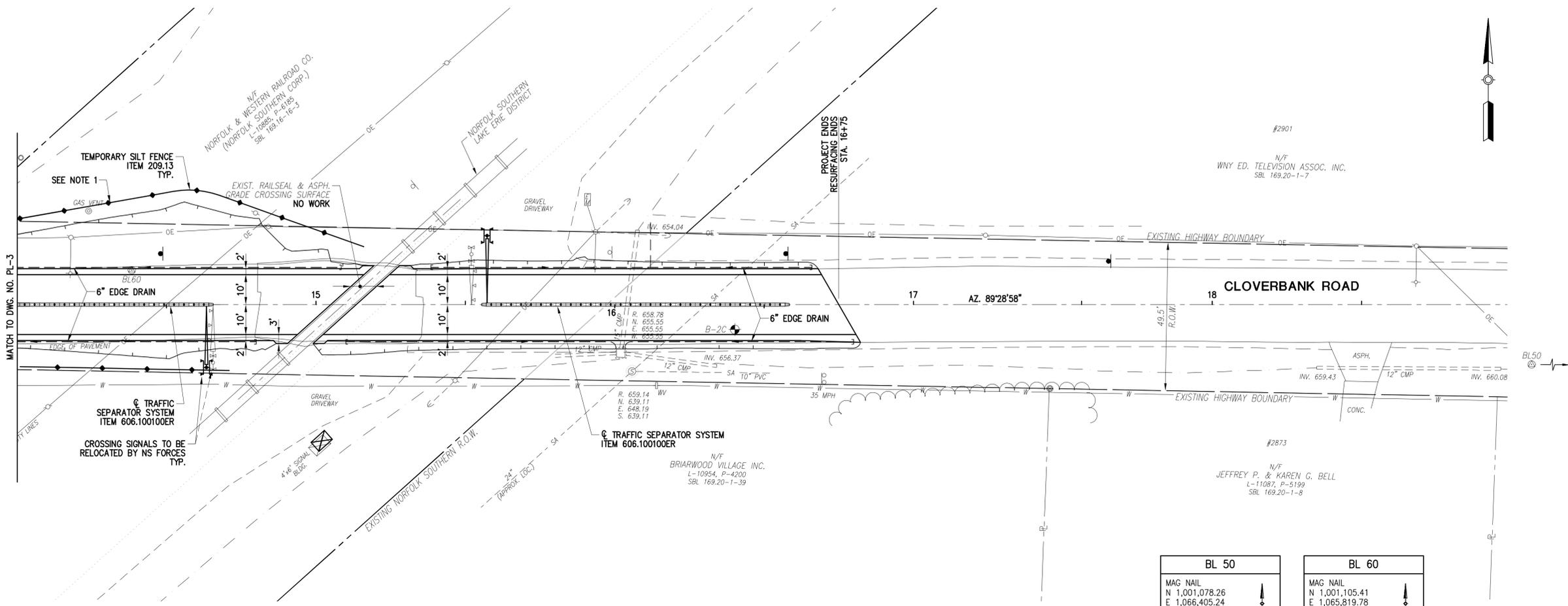


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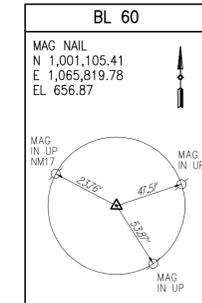
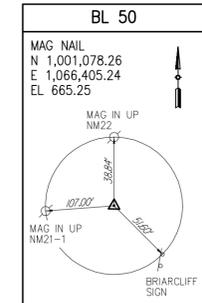
- WORK BEYOND THE EXISTING STREET BOUNDARY & EASEMENTS MAY BE NECESSARY TO RE-ESTABLISH DRIVEWAYS/WALKWAYS, PLANT TREES & SHRUBS, GRADE & RE-ESTABLISH TURF, RE-ESTABLISH UTILITY CONNECTIONS, OR REMOVE/TRIM TREES. REFER TO SPECIAL NOTE 3 IN THE CONTRACT SPECIFICATIONS FOR FURTHER GUIDANCE.
- THE ELECTRIC TRANSMISSION LINE ACCESS DRIVEWAY SHALL BE BROUGHT UP TO MATCH THE PROPOSED EDGE OF PAVEMENT WITH ITEM 623.12 CRUSHED STONE (IN PLACE MEASURE). GRADATION SHALL BE #2 CRUSHER RUN.



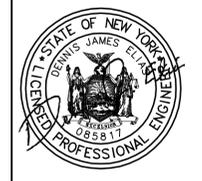
DESIGN SUPERVISOR D. ZIEMANSKI JOB MANAGER J. ZHAO DESIGNED BY J. ZHAO CHECKED BY D. ELIAS ESTIMATED BY D. ELIAS CHECKED BY J. ZHAO DRAFTED BY J. ZHAO CHECKED BY D. ELIAS



NOTES:
 1. WORK BEYOND THE EXISTING STREET BOUNDARY & EASEMENTS MAY BE NECESSARY TO RE-ESTABLISH DRIVEWAYS/WALKWAYS, PLANT TREES & SHRUBS, GRADE & RE-ESTABLISH TURF, RE-ESTABLISH UTILITY CONNECTIONS, OR REMOVE/TRIM TREES. REFER TO SPECIAL NOTE 3 IN THE CONTRACT SPECIFICATIONS FOR FURTHER GUIDANCE.



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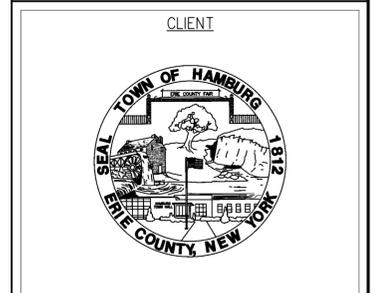


09.03.13
 DATE _____ DATE _____

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REVISIONS			
NO.	DESCRIPTION	BY	DATE

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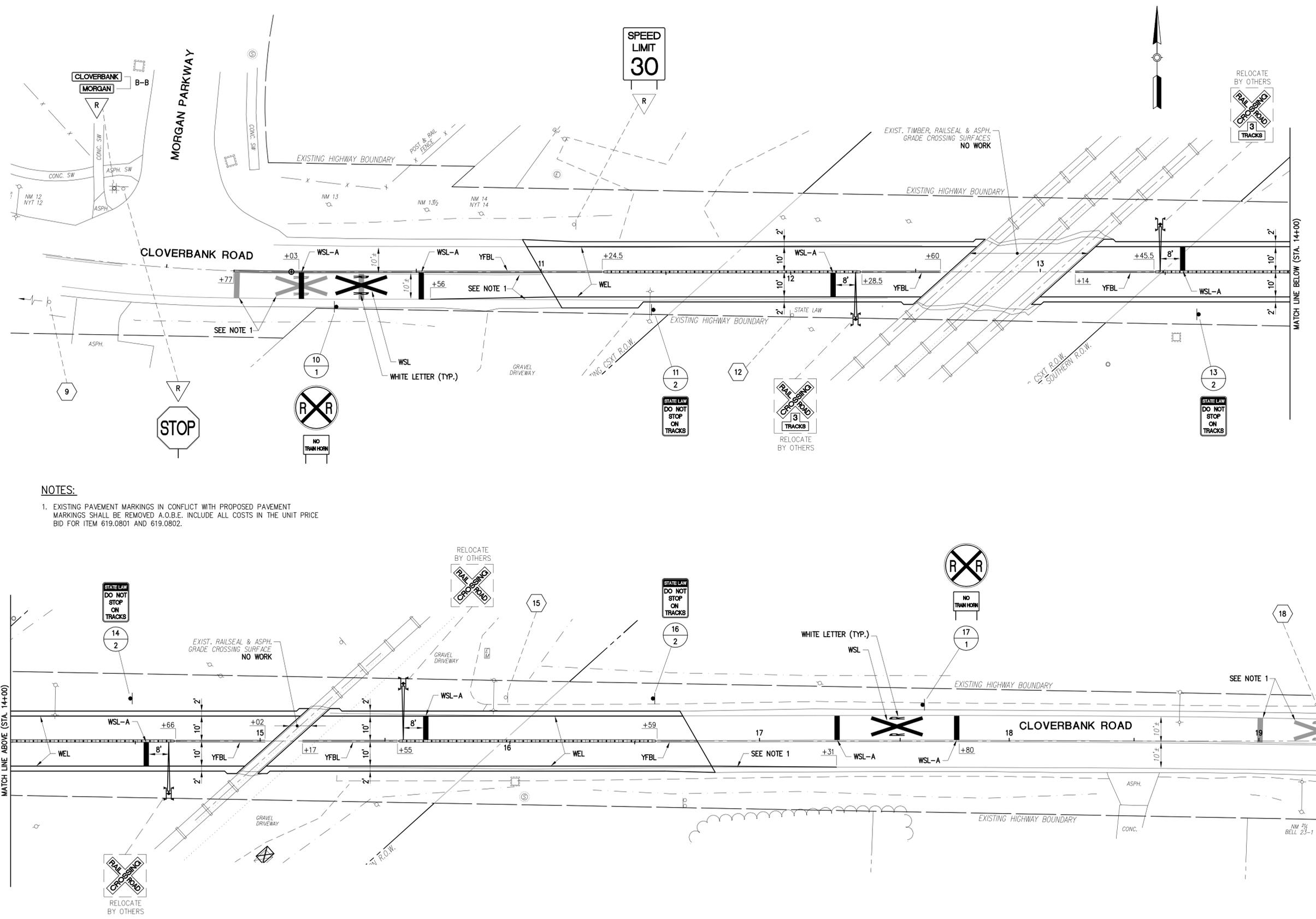


PROJECT NAME
ROGERS ROAD & CLOVERBANK ROAD RAILROAD QUIET ZONES

DRAWING TITLE
CLOVERBANK ROAD PLAN & PROFILE
 STA. 14+00 TO STA. 19+00

SCALE HORIZ: 1" = 20' VERT: 1" = 5'	DATE 9/3/2013
P.I.N. 5759.70	EAA PROJECT NO. 19406.00
SHEET NO. 10 OF 12	DRAWING NO. PL-4

DESIGN SUPERVISOR D. ZIEMANSKI JOB MANAGER D. ELIAS DESIGNED BY J. ZHAO CHECKED BY D. ELIAS ESTIMATED BY J. ZHAO CHECKED BY D. ELIAS DRAFTED BY J. ZHAO CHECKED BY J. ZHAO CHECKED BY D. ELIAS



NOTES:
 1. EXISTING PAVEMENT MARKINGS IN CONFLICT WITH PROPOSED PAVEMENT MARKINGS SHALL BE REMOVED A.O.B.E. INCLUDE ALL COSTS IN THE UNIT PRICE BID FOR ITEM 619.0801 AND 619.0802.

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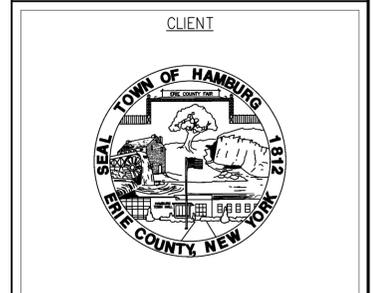


09.03.13
 DATE DATE

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REVISIONS			
NO.	DESCRIPTION	BY	DATE

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CLIENT
ROGERS ROAD & CLOVERBANK ROAD RAILROAD QUIET ZONES

DRAWING TITLE
CLOVERBANK ROAD SIGN & PAVEMENT MARKING PLAN
 STA. 9+00 TO STA. 19+25

SCALE 1" = 20'	DATE 9/3/2013
P.I.N. 5759.70	EAA PROJECT NO. 19406.00
SHEET NO. 12 OF 12	DRAWING NO. PM-2